

VILLAGE OF BARRINGTON HILLS

Roads & Bridges Committee

NOTICE OF MEETING



Tuesday, August 18, 2015 ~ 3:00 pm
112 Algonquin Road

AGENDA

1. Call to Order & Roll Call
2. [Vote] Minutes July 20, 2015
3. Public Comments
4. Discussion Items
 - 4.1 Cuba Road Bridge Update
 - 4.2 2015 Road Program Update
 - 4.3 Old Hart Road Bridge Repairs
 - 4.4 2015 Misc. Drainage Program
 - 4.5 2015-2019 Road Program Planning
 - 4.6 State & County Road Updates
 - 4.7 Functional Classification Status – Ridge & Plum Tree
 - 4.8 Lake View Lane Drainage
 - 4.9 Enbridge Energy Line 6A Maintenance - Spring Creek Road
5. Adjournment

Chairman: Brian Cecola

NOTICE AS POSTED

112 Algonquin Road ~ Barrington Hills, IL 60010-5199 ~ 847.551.3000

VILLAGE OF BARRINGTON HILLS
Roads & Bridges Committee
Meeting Minutes
July 20, 2015



Committee Members Present: President Martin McLaughlin
Trustee Brian Cecola, Chair
Robert Kosin, Director of Administration
Dan Strahan, Village Engineer

1. ORGANIZATIONAL: The meeting of the Village of Barrington Hills Roads & Bridges Committee was called to order at 2:58 p.m. by Chairman Cecola.

2.1 APPROVAL OF MINUTES: The minutes of the Roads & Bridges Committee Meeting of June 16, 2015 were approved as written.

3. PUBLIC COMMENTS: None

4.1 CUBA ROAD BRIDGE UPDATE: Mr. Strahan reviewed the memo included in the packate dated July 17, 2015, noting the August 5th deadline to complete the certification of ROW. President McLaughlin noted that when the schedule is finalized communications should go out to residents as well as Cuba Township regarding closure of the bridge.

4.2 LAKEVIEW LANE DRAINAGE UPDATE: Mr. Strahan reviewed the preliminary engineering work done to date and potential sizing of a storm sewer. It was noted that typically the Village would participate in significant drainage projects of this type only when a Village road was overtopping or flooding was impacting residences. After further discussion Village staff was directed to meet with Mr. Keady to provide recommendations on how to pursue private drainage improvements.

4.3 VILLAGE OF ALGONQUIN – HIGHLAND/SPRING CREEK IGA STATUS UPDATE: It was noted that no response had been received to date from the Village of Algonquin to staff's meeting request. Bob Kosin indicated he would contact the Village Administrator to arrange a meeting.

4.4 CUBA TOWNSHIP SERVICES: Trustee Cecola noted he had made several attempts to contact Cuba Township to review a scope of services. The Committee discussed a variety of Township Services that may be available to the Village, including brush clearing, cold patching, and surface patching. Mr. Kosin noted that a pricing list should be provided by the Township to define the potential costs for the services offered.

4.5 VILLAGE HALL PARKING LOT STRIPING QUOTES: Mr. Strahan noted that two additional quotes had been requested and one quote was received to supplement the first quote received in June to restripe the Village Hall parking lot. The

additional quote was received from Marking Specialists in the amount of \$4,600. After discussion it was determined that the quote from Marking Specialists would be presented to the Village Board for consideration.

4.6 SOLAR MOBILE SPEED SIGN: Mr. Strahan reviewed the packet materials which included research of options for potential replacements to the solar mobile speed sign trailer. Research demonstrated that radar speed sign technology lowers traveled speeds. After discussion Trustee Cecola requested that staff review potential locations for application of the technology.

4.7 ROADSIDE MOWING : Mr. Strahan noted that roadside mowing was completed the week of June 15th and that depending upon weather an additional mowing may be needed later in the summer.

4.8 COOK COUNTY ROADS & RESURFACING UPDATE: Mr. Strahan noted that the Cook County Highway Department had confirmed that Otis Road would be resurfaced from Brinker Road to Dundee Lane as part of their 2015 resurfacing program. Trustee Cecola requested that a meeting be arranged with the Cook County Highway Department to review their criteria for resurfacing projects and their anticipated schedule for other roads in the Village.

5. ADJOURNMENT: The meeting was adjourned at 3:45 PM.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 14, 2015

Re: Cuba Road Bridge Replacement Project
Status Update

The following is a summary of the current status of the Cuba Road Bridge project.

Land Acquisition & Schedule

We anticipate the project will be posted in the IDOT Service Bulletin on Friday, August 14, 2015 as Contract No. 61B04. Bids will be opened on September 18, 2015, and the approximate schedule is as indicated below:

- Letting Date/Bid Opening September 18, 2015
- Approximate Construction Start Date November 15, 2015
- Work on Structural Items November-February as weather allows
- Projected Bridge Opening Date May 13, 2016*

*15 working days will be permitted after May 13, 2016 to complete roadway and landscaping items.

The Village has issued checks for the land acquisition required for the bridge replacement project. Mathewson ROW Company will issue the checks to the property owners pending completion of certain remaining lending requirements.

Utility Relocation

ComEd has completed the pole relocations necessitated by the bridge replacement. This past week our office responded to concerns from the adjacent property owner regarding the location of the poles as well as brush and debris left on the property by ComEd. Permit authorizations have been issued to Nicor and AT&T for the work required to relocate their facilities in the area. It is anticipated that the utility work will be completed well ahead of the proposed construction start this fall.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 14, 2015

Re: 2015 Road Program

On Tuesday, August 11, 2015 a preconstruction meeting was held with Geske & Sons, Inc. in anticipation of the start of construction operations for the Village's 2015 Road Program. Geske & Sons, Inc. was awarded the project at the May 2015 Board meeting, to include the patching and resurfacing of Meadow Hill Road (north of Fox Hunt Lane), Haegers Bend Road (County Line Road to Chapel Road), and Plum Tree Road (Ridge Road west to the Village limits), as well as patching on Ridge Road and Helm Road.

Contract documents have been executed and approved by IDOT and the contractor anticipates beginning work the week of August 17, 2015. Initial construction operations would include spot removal and replacement of concrete curb & gutter and culvert replacements.

Notice of the start of construction will be sent to residents along the affected roadways and also posted to the Village website.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 13, 2015

Re: Old Hart Road Bridge Repair

This past month Wiss Janney Elstner Associates, Inc. completed a structural inspection of the Old Hart Road Bridge. While the bridge is in fair condition, the report notes concrete deterioration along the east and west edge of the deck slab and recommends that concrete repairs be performed. The inspection report and photos is attached.

We would recommend that concrete repairs be completed at this location under the Drainage Management portion of the Roads & Bridges budget. Similar repairs were completed at the Green Rail Bridge on Oak Knoll last year for less advanced concrete deterioration in the amount of \$17,300. The anticipated cost for the bridge repairs proposed is approximately \$35,000-\$50,000. If directed by the Roads & Bridges Committee construction documents will be prepared and a bid advertisement issued per the schedule outlined below:

- Bid Advertisement – September 3, 2015
- Bid Opening- September 22, 2015
- Contract Award- September 28, 2015
- Construction- October/November 2015

Via Email: dstrahan@gha-engineers.com

July 29, 2015

Mr. Daniel Strahan
Assistant Village Engineer - Village of Barrington Hills
Gewalt Hamilton Associates, Inc.
625 Forest Edge Drive
Vernon Hills, IL 60061

Re: Old Hart Road Bridge Inspection
Village of Barrington Hills
WJE No. 2015.3685

Dear Mr. Strahan:

Wiss, Janney, Elstner Associates, Inc. (WJE) recently completed the inspection of the Old Hart Road Bridge over Flint Creek. The inspection was performed on July 17, 2015 in accordance with the National Bridge Inspection Standards (NBIS), even though this bridge structure does not meet the NBIS criteria for submittal to the Illinois Department of Transportation (IDOT) based on its clear span of 18 feet.

The bridge is located on Old Hart Road, approximately 300 feet north of Oak Knoll Road. The bridge was constructed using a conventionally reinforced concrete deck slab, approximately 11 inches thick and 21 ft-3 in. wide. The bridge deck is oriented in a north south direction and carries two 10-foot lanes. The asphalt approach pavements are continuous with the asphalt overlay on the bridge deck. Reinforced concrete parapets are located along each edge of the bridge deck. Figure 1 shows the roadway looking south. Figure 2 is an elevation view of the bridge from the creek looking east. The north and south abutments are constructed using reinforced concrete with integral wingwalls, also shown in Figure 2. The year of construction is unknown. At the time of our inspection, the average water depth was approximately 2 to 3 feet.

Condition Survey

Overall the bridge structure was observed to be in fair condition. The soffit of the bridge deck exhibited areas of concrete deterioration, which was typically located along the east and west edges of the deck slab. The most advanced concrete deterioration was noted below the east bridge rail, as the entire eastern edge of the deck soffit is spalled or delaminated for a width of approximately 1 ft-6 in. to 4 ft-6 in. from the edge of the deck. In addition, multiple reinforcing steel bars in this area are exposed and corroded, as shown in Figure 3. The west edge of the bridge deck exhibits similar delaminations and freeze-thaw deterioration that extends between approximately 1 ft-6 in. and 3 ft from the deck edge; however, the concrete has not yet spalled off the deck. The deterioration of along the west edge of the bridge deck is shown in Figure 4. It should be noted that the reinforced concrete parapet is connected to the deck slab with hooked reinforcing steel bars along the edges of the bridge deck. These hooked reinforcing bars were observed to be corroded and no longer effective along the east edge of the deck. The remaining areas of the concrete deck soffit, between the deteriorated edges, were generally sound and no flexural or shear cracks were noted.

On the top surface of the deck, an area of deterioration, or possible impact damage, was observed on the south face of the east parapet. Minor scaling was also noted on the east parapet at the roadway shoulder.

Minor vegetation growth is typical along both bridge rails, at the joint between the asphalt roadway and the concrete bridge rail. A longitudinal crack is present in the asphalt overlay at the centerline of the bridge deck, and several transverse cracks are present in the asphalt overlay, as shown in Figures 1 and 5. In addition, slight depressions were observed in the approach pavement, with the largest being at the south approach pavement, near the east edge, which is also shown in Figure 5. This condition is indicative of movement of fill behind the south abutment. A roadway inspection opening was not made during this inspection, so the asphalt depth and condition of the top surface of the reinforced concrete deck slab could not be determined. The existing bridge rails do not conform to current IDOT standards as there are no approach guard rails or transitions to the bridge parapets.

The northwest and northeast wingwalls typically exhibited freeze/thaw deterioration and cracking with efflorescence, as shown in Figures 6 and 7. In addition, isolated areas of poor concrete consolidation and cracking with efflorescence were observed at various locations on the abutments and wingwalls, as shown in Figure 8. Isolated areas of concrete delaminations are also present on each wingwall, which is also illustrated in Figure 8.

Lastly, a small amount of debris has accumulated in the waterway at the southeast corner of the bridge. Stream migration to the north was noted upstream of the bridge.

Recommendations

The concrete that comprises the bridge deck (superstructure), rails, and substructures exhibit varying degrees of concrete deterioration and freeze-thaw damage. Currently, the deterioration along the east edge of the bridge deck have compromised the load-carrying capacity of the structural slab. Therefore, it is recommended that concrete repairs be performed to the concrete deck soffit along the entire east and west edges of the bridge. A schematic of the repair (Sketch A-1) has been included in Appendix A. The repair sequence for this work would be as follows:

1. Temporarily support/shore the bridge rail above the area to be repaired to stabilize the concrete rail vertically and laterally.
2. Chip existing deteriorated/delaminated concrete slab to the extents shown in the Sketch A-1.
3. Sandblast the exposed reinforcing steel and concrete surfaces.
4. Coat the existing reinforcing steel with an epoxy paint.
5. For the existing reinforcing steel that exhibits section loss, supplement with new #4 or #6 epoxy-coated reinforcing bars. Area of new reinforcing bars shall be at least be equal to the worst case section loss of the existing bars. Total section loss for the three existing reinforcing bars may be added together in order to provide one or two new supplemental reinforcing bar(s). The new epoxy-coated reinforcing bars shall extend a minimum of 18 inches past the area of section loss on each side, but need not exceed a length of 18 feet (clear span of bridge).
6. Epoxy dowel new reinforcing steel bars into the existing deck slab (horizontally) and the bridge rail (vertically), as shown schematically in Sketch A-1.
7. Restore the existing cross-section using shotcrete or a form-and-pour repair with flowable concrete and an adequately sized bird's mouth on the side of the forms.
8. Apply a silane sealer to the new concrete in accordance with the manufacturer's recommendations.

The deterioration of the slab edge has also significantly compromised the connection between the deck slab and the bridge rails, particularly along the east edge. The schematic repair shown in Appendix A provides dowels in an effort to supplement this deteriorated connection. These dowels will help to restore the bridge rail connection; however, the capacity of the repaired bridge rail is not known without performing additional investigation and analysis. It should be noted that a crash tested bridge rail would not be required for this

structure, as the Annual Average Daily Traffic (AADT) is less than 1,000 vehicles. If the proposed repair, shown in Appendix A, will not be performed in the near future, the Village may wish to consider the installation a temporary barrier along the east edge of the deck to protect the east bridge rail from vehicle impact damage.

A total of approximately 175 sq. ft. of deterioration (total) is present along the edges of the concrete bridge deck. The deterioration along the east and west edges of the bridge deck is likely due to moisture entry along the bridge rail. As a result, the application of a hot-applied crack filler/sealer is also recommended along the asphalt/bridge rail joint, as well as in the cracks present in the asphalt overlay.

Other, lower-priority, maintenance items include concrete repairs at each of the wingwalls and the application of a silane sealer to the surfaces of the abutments, wingwalls, and bridge rails. Currently, approximately 20 sq. ft. of concrete deterioration/delaminations are present on the above water portion of each wingwall.

Conditions of stream migration upstream of the bridge should be monitored. The small amount of debris beginning to accumulate in the waterway should also be monitored to prevent the back up of water behind the bridge.

Summary and Conclusions

WJE engineers completed an inspection of the Old Hart Road Bridge over Flint Creek. This bridge structure is in fair condition, but requires short-term concrete repairs to the edges of the concrete deck slab to restore the load-carrying capacity of the bridge deck. This structure does not meet the NBIS criteria for submittal to the Illinois Department of Transportation (IDOT); however, the structure should be inspected routinely to assess its condition.

Please call if you have any questions or require further information.

Sincerely,

WISS, JANNEY, ELSTNER ASSOCIATES, INC.



Douglas D. Crampton, P.E., S.E.
Associate Principal
Licensed Structural Engineer
Illinois No. 6108



FIGURES



Figure 1. View of Old Hart Road Bridge over Flint Creek looking south.



Figure 2. Bridge elevation looking east.



Figure 3. Spalled concrete and exposed reinforcing bars along east edge of deck soffit.



Figure 4. Spalled concrete and efflorescence (arrows) at southwest corner of the deck soffit (red line shows extent of delaminated concrete).



Figure 5. Settlement of approach pavement at the southeast corner of the bridge deck (arrow) and transverse cracking of the asphalt pavement (arrows).



Figure 6. Freeze/thaw damage on the northwest wingwall, including cracking with efflorescence.

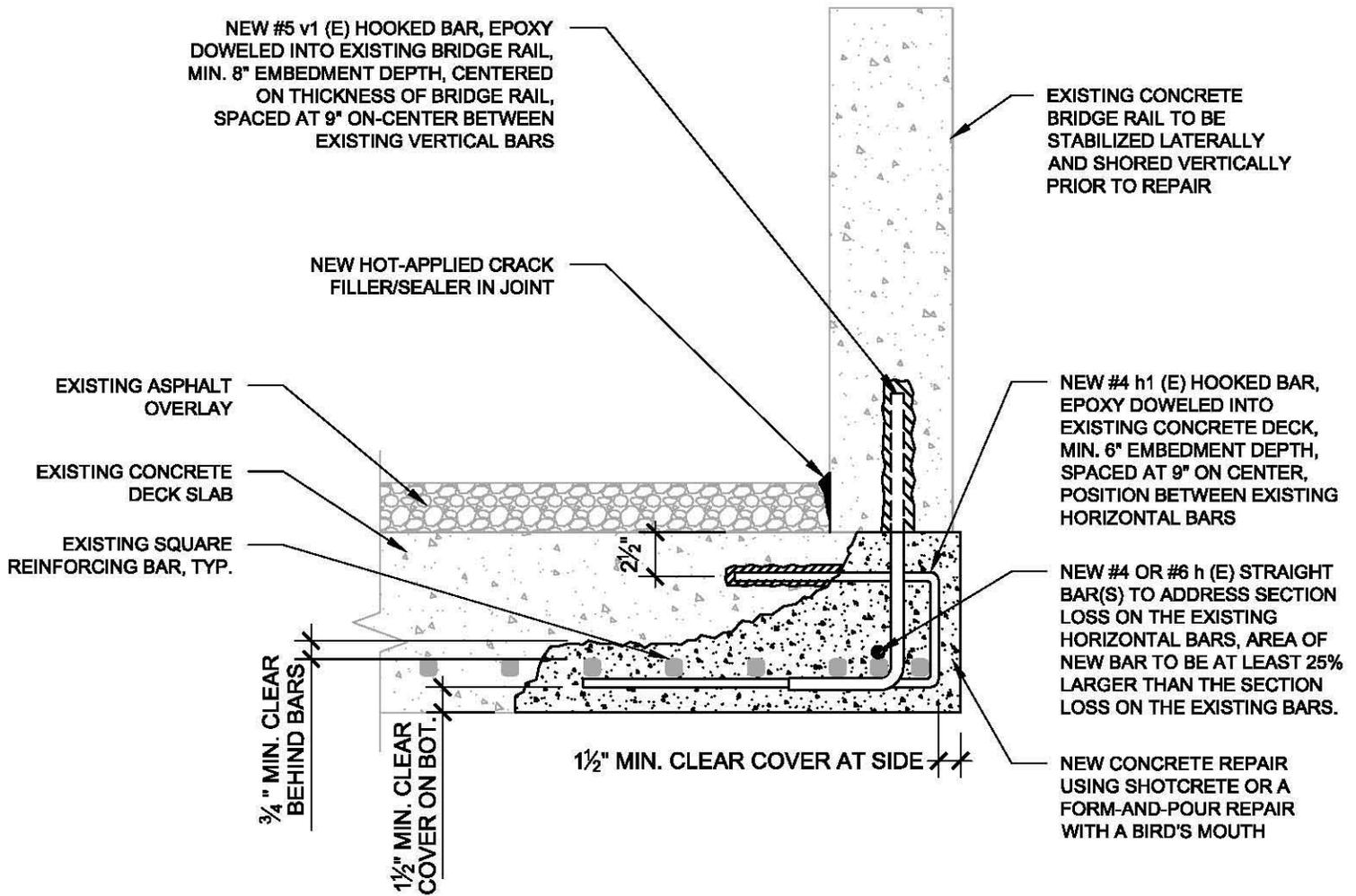


Figure 7. Freeze/thaw damage at the top of the northeast wingwall highlighted by cracking with efflorescence.



Figure 8. South abutment showing areas of efflorescence (arrows) and delamination (circle).

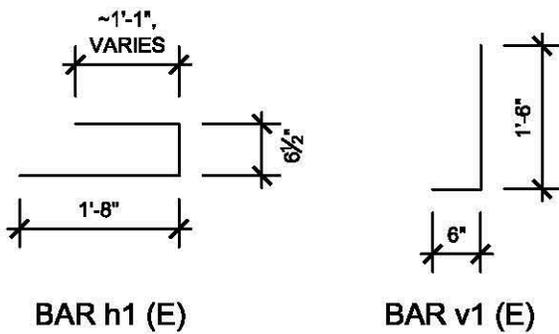
APPENDIX A - CONCRETE REPAIR SCHEMATIC FOR THE DECK EDGE



A-1 SECTION THROUGH SLAB EDGE
SCALE: 1" = 1'-0"

BILL OF MATERIALS

Bar	No.	Size	Length	Shape
h (E)	4*	#4, #6*	18'-0"*	—
h1 (E)	48	#4	3' - 4"	⌊
v1 (E)	48	#5	2' - 0"	⌋
Concrete Repair			Sq. ft.	175
Reinforcement Bars Epoxy Coated			Pound	315



*** INDICATES THAT THE EXACT DIMENSIONS ARE UNKNOWN UNTIL THE CONCRETE DETERIORATION IS REMOVED AND THE REPAIR PROFILE IS DETERMINED. APPROXIMATE DIMENSIONS AND QUANTITIES SHOWN.

NOTES:

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION *STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION*, CURRENT EDITION
2. ALL MATERIALS AND PROCEDURES SHALL BE APPROVED BY THE ENGINEER PRIOR TO STARTING THE WORK.
3. REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
4. EXISTING CONCRETE SURFACES AND REINFORCING BARS TO BE SANDBLASTED CLEAN PRIOR TO REPAIR.
5. EXISTING REINFORCING BARS SHALL BE PAINTED WITH AN EPOXY PAINT AFTER SANDBLASTING.
6. NEW REINFORCING BARS TO BE ASTM A615 GRADE 50.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 13, 2015

Re: Drainage Maintenance Work Items

Over the past several months the Village has received several requests to address minor drainage issues at various locations. These areas have been reviewed in past Roads & Bridges Committee meetings but include the following:

- Steeplechase Road – Ditch Grading
- 35 Dundee Lane – Ditch Grading
- Hills & Dales Road – Shoulder Erosion
- Lakeview Lane – Ditch grading and/or culvert repairs

As noted under agenda item 4.3, repairs to the Old Hart Road Bridge are also recommended to be completed before the end of this construction season. Consideration can be given to creating a bid package for the scope of work outlined above or requesting costs from Cuba Township as general roadway maintenance items. As outlined during the June Roads & Bridges meeting it is anticipated that approximately \$50,000 will remain in the Drainage Management portion of the Roads & Bridges budget after reimbursement is received for the Merri Oaks Road Drainage improvements.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 14, 2015

Re: Road Program Planning

Over the next few months the Village Board will review and consider a proposed budget for 2016. In anticipation of the budget process it would be appropriate to resume discussions that had begun last spring regarding the Village's approach to a multi-year road program.

After initial presentation of two potential five year resurfacing programs (2015-2019), several suggestions were made regarding both the duration of the plan as well as the annual funding. To aid in review of this topic, I have attached the most recent summary documentation from February which included six different options. When considering various funding options, it is noted that 2015 resurfacing costs are approximately \$297,000 per mile based on the approved low bid. The table below illustrates the average lifespan required of Village roadways depending upon the level of funding established, not accounting for inflation. Please note our recommendation is to resurface a minimum of 3.2 miles of roads per year, which would result in an average surface life of 12 years.

Average "Road Maintenance Contracts" Budget Amount (2015 Dollars)	Miles of Road Resurfacing	Average Pavement Lifespan
\$800,000	2.69	14.5
\$900,000	3.03	12.9
\$1,000,000	3.37	11.6
\$1,100,000	3.70	10.5

As a reminder, this calculation represents a single line item (Road Maintenance Contracts) within the Roads & Bridges budget. Below is the overall Roads & Bridges budget approved for 2015.

		2015 BUDGET
50701	ROAD MAINTENANCE CONTRACTS	\$ 986,846.00
50702	SNOWPLOWING CONTRACTS	260,000.00
50703	MOWING/CLEANUP CONTRACTS	35,000.00
50704	SIGN PURCHASE/INSTALLATION	14,000.00
50705	DRAINAGE MANAGEMENT	120,000.00
50706	ENGINEERING FEES	180,000.00
50707	ROAD STRIPING	1,000.00
50708	EQUIPMENT MAINTENANCE	4,000.00
50709	ROAD PATCHING CONTRACTS	15,000.00
50710	EQUIPMENT PURCHASES	2,000.00
50711	BRIDGE INSPECTIONS	8,000.00
50713	CUBA ROAD BRIDGE RESTORAL EXP	<u>160,000.00</u>
	TOTAL EXPENDITURES	\$ 1,785,846.00

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Patty Meroni, VBH Chairman Roads & Bridges

From: Daniel J. Strahan, P.E., CFM
Gewalt Hamilton Associates

Date: February 20, 2015

Re: Multi-Year Road Program

At the January 2015 Village Board meeting, the Board discussed the 2014 Pavement Condition Study. The study had included two options to consider for a five-year plan to approach the roadway improvements, covering the years 2015-2019. After discussion regarding the length and approach of the proposed road program at the January meeting, the Board requested the presentation of additional options to consider at the February meeting.

We have included for the Board packet six options for consideration of the Board, which were developed based on the two options initially presented for a five-year road program. Options 1A, 1B, and 1C each target 3.2 miles of resurfacing per year over a period of 5-years, 7-years, and 10-years, respectively. Options 2A, 2B, and 2C include an accelerated resurfacing approach to eliminate approximately 4.5 miles of "backlog" due to deferred maintenance over the last several years, again using plan durations of 5-years, 7-years, and 10-years respectively. The following is noted as the board considers the various plan options:

- **Jurisdiction-** There is often confusion regarding which roads located within Barrington Hills are maintained by the Village. A list of every road within the Village is attached for reference noting the various jurisdictions involved. The plans have been prepared based on the 39 miles of road maintained by Barrington Hills, and do not include those roads maintained by IDOT, the counties, or privately maintained roads.
- **Approach-** As noted, we have suggested two potential resurfacing approaches: (1) Resurface an average of 3.2 miles of roadway per year to maintain current overall pavement conditions, and (2) Pursue an accelerated approach to improve overall pavement conditions. Preliminary budget amounts are shown for each option, both in 2015 dollars and adjusted for inflation. For reference the 2015 budget line item for Road Maintenance Contracts includes approximately \$986,000 for the road resurfacing program.
- **Plan Duration-** Each approach is shown carried out through a 5-year, 7-year, or 10-year time frame based on the discussion of the Board in January. As noted previously, we had recommended a 5-year plan based on our experiences having to consistently revisit and modify the final years of a 10-year plan. If a longer range plan is desired we would reiterate that it should be considered a flexible planning document, especially in the later years of the plan.

**Village of Barrington Hills
Roadway List by Jurisdiction**

<u>Name</u>	<u>Length</u>	<u>From</u>	<u>To</u>	<u>Jurisdiction</u>	<u>County</u>
Bateman Road	3.7	Penny Road North	Lake Cook Road	County	Cook
Brinker Road	3	Lake Cook Road South	Algonquin Road	County	Cook
Donlea Road	1.4	Lake Cook Road South	Sutton Road	County	Cook
Old Sutton Road	4.25	Lake Cook Road South	N. of Penny Road	County	Cook
Otis Road	2.7	Old Sutton Road East	Dundee Avenue	County	Cook
Palatine Road	1.4	Stover Road East	Village Limits	County	Cook
Penny Road	2.4	Old Sutton Road West	Village Limits	County	Cook
TOTAL:	18.9				

<u>Name</u>	<u>Length</u>	<u>From</u>	<u>To</u>	<u>Jurisdiction</u>	<u>County</u>
Algonquin Road (62)	5.2	Village Limits West	Elgin Road (25)	State	Cook, Kane
Barrington Road	0.25	Village Limits South	Palatine Road	State	Kane
Dundee Road	5.5	Barrington Road West	Village Limits	State	Cook
Elgin Road (25)*	0.8	Algonquin Road (62) South	Bolz Road	State	Cook
Hawthorne/New Sutton Road (59)	2.5	Dundee Lane South	Village Limits	State	Cook
Lake Cook Road*	5.6	Haeger's Bend Road East	Village Limits	State	Cook, Lake
TOTAL:	19.9				

*Note: Jurisdiction of County Line Road west of power lines and Elgin Road North of IL Rte 62 is McHenry County DOT

<u>Name</u>	<u>Length</u>	<u>From</u>	<u>To</u>	<u>Jurisdiction</u>	<u>County</u>
Aberdeen Drive	0.2	Crabapple Road	East & West	Village	Cook
Barrington Hills Road	0.5	Donlea Road North	End	Village	Cook
Butternut Road	0.4	Donlea Road South	End	Village	Cook
Caesar Drive	0.8	Lake Cook Road South	End	Village	Cook
Country Oaks Drive	0.6	Lake Road Road North	End	Village	McHenry
Country Oaks Lane	0.3	Country Oaks Drive West	End	Village	McHenry
Crabapple Road	0.3	Donlea Road South	End	Village	Cook
Creekside Lane	1.2	Old Sutton Road East	in Cricle	Village	Cook
Cross Timbers Road	0.3	Braeburn Road East	End	Village	McHenry
Dana Lane	0.2	Caesar Drive West	End	Village	Cook
Healy Road	0.7	Penny Road	Village Limits	Village	Cook
Hickory Lane	0.2	Merri Oaks Road North	End	Village	Lake
Honeycut Road	0.2	Hills & Dales Road South	End	Village	Cook
Lake View Lane	0.4	Dundee Road (68) North	End	Village	Cook
Leeds Drive	0.2	Crabapple Road West	End	Village	Cook
Little Bend Road	0.7	Spring Lane	End	Village	McHenry
Longmeadow Court	0.1	Longmeadow Drive North	End	Village	Cook
Longmeadow Drive	0.4	Bateman Road West	Rolling Hills Drive	Village	Cook
Old Bartlett Road	0.2	Bartlett Road	Cul-de-Sac	Village	Cook
Old Dundee Road	1.4	Routes 58 & 68 South	Cul-de-Sac	Village	Cook
Rebecca Drive	0.3	Old Sutton Road West	End	Village	Cook
Rock Ridge Road	0.5	Plum Tree Road South	End	Village	McHenry
Rolling Hills Drive	0.6	Bateman Road West	Longmeadow Drive	Village	Cook
Round Barn Road	0.2	Hawley Woods Road East	End	Village	Cook
Spring Lane	1	Spring Creek Road	End	Village	McHenry
Springwood Lane	0.2	Algonquin Road (62) North	End	Village	Cook
Steeplechase Road	1.1	Lake Cook Road North	in Cricle	Village	Lake
Surrey Court	0.1	Surrey Lane West	End	Village	McHenry
Surrey Lane East	0.1	Surrey Lane West	Wagon Wheel	Village	McHenry
Surrey Lane West	0.7	Plum Tree Road North	End	Village	McHenry
Tamarack Lane	0.2	Old Sutton Road West	End	Village	Cook
Three Lakes Road	0.2	Lake Cook Road South	End	Village	Cook
Tricia Lane	0.2	Old Sutton Road East	End	Village	Cook
Wagon Wheel Lane	0.2	Surrey Lane East	Surrey Lane West	Village	McHenry
Woodcreek Road	0.5	Dundee Road (68) South	End	Village	Cook
Braeburn Road	1.3	Spring Creek Road South	Plum Tree Road	Village	McHenry
Buckley Road	0.8	Cuba Road South	Oak Knoll Road	Village	Lake
Chapel Road	1	Haeger's Bend Road East	Church Road	Village	McHenry
Church Road	0.7	Chapel Road North	River Road	Village	McHenry
Cuba Road	0.7	Merri Oaks	Plum Tree Road	Village	Lake
Dundee Lane	1	Dundee Road (68) North	Route 59	Village	Cook
Hawley Woods Road	1.1	Otis Road South	Old Dundee Road	Village	Cook
Hawthorne Lane	0.4	Route 59 North	Otis Road	Village	Cook
Healy Road	0.8	Dundee (48) Road	Penny Road	Village	Cook
Hills & Dales Road	0.8	Brinker Road East	Otis Road	Village	Cook
Meadow Hill Road	1	Lake Cook Road North	Spring Creek Road	Village	McHenry
Merri-Oaks Road	0.7	Ridge Road East	Cuba Road	Village	Lake
Oak Knoll Road	2.4	Old Hart Road West	Ridge Road	Village	Lake
Old Hart Road	0.6	Lake Cook Road North	End	Village	Lake
Spring Creek Road	3.3	Ridge Road West	Village Limits	Village	McHenry
Haeger's Bend Road	2	Lake Cook Road North	Village Limits	Village	McHenry
Helm Road	0.8	Algonquin Road (62) West	Village Limits	Village	Cook, Kane
Plum Tree Road	1.7	C & NW Railroad West	Village Limits	Village	Lake, McHenry
Ridge Road	2	Lake Cook Road North	Plum Tree Road	Village	Lake, McHenry
River Road	0.5	Haeger's Bend Road South	Village Limits	Village	McHenry
TOTAL:	39.0				

<u>Name</u>	<u>Length</u>	<u>From</u>	<u>To</u>	<u>Jurisdiction</u>	<u>County</u>
Abbey Woods Lane	0.1	Barrington Road	End	Private	Cook
Abbey Woods Drive	0.2	Abbey Woods Lane	End	Private	Cook
Acorn Lane	0.2	Merri Oaks Road South	End	Private	Lake
Alderberry Lane	0.1	Church Road East	End	Private	McHenry
Ascot Lane	0.3	Spring Creek Road North	End	Private	McHenry
Ashbury Lane	0.5	Lake Cook Road North	in Circle	Private	McHenry
Auburn Lane	0.2	Spring Creek Road South	End	Private	McHenry
Autumn Trail	0.7	Algonquin Road (62) South	in Circle	Private	Kane
Barrington Bourne	0.5	Lake Cook Road North	in Circle	Private	McHenry
Bateman Circle	0.9	Bateman Road West	in Circle	Private	Cook
Bellwood	0.5	Lake Cook Road North	in Circle	Private	Lake
Berron Lane	0.4	Bateman Road	End	Private	Cook
Bisque Drive	0.1	Paganica Drive West	End	Private	Lake
Bow Lane	0.8	Spring Creek Road South	Meadow Hill Road	Private	McHenry
Braeburn Lane	0.6	Spring Creek Road South	End	Private	McHenry
Burning Oak Trail	0.4	Braeburn Road West	in Circle	Private	McHenry
Crawling Stone Road	0.3	Deepwood Road North	End	Private	Cook
Deepwood Court	0.1	Deepwood Road West	End	Private	Cook
Deepwood Road	2	Batemen Road North	Lake Cook Road	Private	Cook
Dormy Lake	0.2	Paganica Drive East	End	Private	Lake
Dunrovin Drive	0.35	Steeplechase Road North	End	Private	Lake
Eagle Pointe Drive	0.25	Bateman Road West	End	Private	Cook
East Lane	0.1	Hawley Woods Road East	End	Private	Cook
Far Hills Road	0.5	Deepwoods Road North	in Circle	Private	Cook
Fernwood Drive	0.25	Bateman Road West	End	Private	Cook
Fox Hunt Road	0.25	Meadow Hill Road East	End	Private	Lake
Goose Lake Drive	0.6	Brinker Road West	End	Private	Cook
Hart Hills Road	0.15	Oakdene Road West	End	Private	Cook
Heron Lane	0.1	Otis Road South	End	Private	Cook
Hubbell Court	0.1	Marbury Lane East	End	Private	Cook
Jacqueline Drive	0.25	Asbury Avenue South	End	Private	McHenry
Jane Lane	0.3	Plum Tree Road North	End	Private	McHenry
Jennifer Court	0.4	Old Dundee North	End	Private	Cook
Juliano Court	0.1	Thornhill Drive	End	Private	Cook
King Road	0.3	Penny Road South	End	Private	Cook
Kresmery Lane	0.15	Plum Tree Road North	End	Private	McHenry
Magnuson Court	0.15	Pondgate Drive	End	Private	Cook
Marbury Lane	0.2	Dundee Lane East	Hubbell Court	Private	Cook
Marmon Lane	0.2	Oakdene Road West	End	Private	Cook
Mid Oaks Lane	0.1	Oak Knoll Road South	End	Private	Lake
Middlebury Road	0.2	Crawling Stone Road East	End	Private	Cook
Moate Lane	0.35	Spring Creek Road North	End	Private	McHenry
Oak Lake Drive	0.15	Merri Oaks Road South	End	Private	Lake
Oak Wood Drive	0.2	New Hart Road South	End	Private	Lake
Oakdene Drive	0.2	Oakdene Road West	End	Private	Cook
Oakdene East	0.15	Oakdene Road	End	Private	Cook
Oakdene Road	0.7	Lake Cook Road North	Windrush Lane	Private	Cook
Oakdene West	0.6	Oakdene Road	End	Private	Cook
Overlook Road	0.1	Far Hills Road East	End	Private	Cook
Paganica Drive	0.35	Oak Knoll Road North	End	Private	Lake
Peraino Circle	0.9	Peraino Drive North	In Circle	Private	Lake
Peraino Drive	0.1	Ridge Road East	Periano Circle	Private	Lake
Pheasant Drive	0.3	Ridge Road East	End	Private	Lake
Pondgate Drive	0.8	Penny Road	End	Private	Cook
Porter School Road	0.2	Buckley Road West	End	Private	Lake
Potter Lane	0.1	Dundee Road South	End	Private	Cook
Raintree Place	0.2	Church Road East	End	Private	McHenry
Regan Boulevard	0.2	Algonquin Road (62) South	End	Private	Kane
Remington Drive	0.45	Bateman Road	End	Private	Cook
Ridgecroft Lane	0.7	Meadow Hill Road East	Spring Creek Road	Private	McHenry
Roundstone Lane	0.5	Helm	End	Private	Cook
Royal Way	0.3	Algonquin Road (62) South	in Circle	Private	Kane
Rub-of-Green Lane	0.1	Bisque Drive	North & South	Private	Lake
Sandalwood Drive	0.3	Oakdene Road East	End	Private	Cook
Sara Lane	0.1	Dundee Lane East	End	Private	Cook
Saville Row	0.3	Braeburn Road East	End	Private	McHenry
Sieberts Ridge Road	0.2	Cuba Road South	End	Private	Lake
Valley Drive	0.3	Otis Road North	End	Private	Cook
Wesley Court	0.1	Pondgate Drive	End	Private	Cook
West Lane	0.1	Hawley Woods Road	End	Private	Cook
Westfield Way	0.4	Remington	End	Private	Cook
Windrush Lane	0.3	Otis Road North	End	Private	Cook
Woodhaven Lane	0.5	Meadow Hill Road West	End	Private	McHenry
Woodrock Road	0.15	Far Hills Road West	End	Private	Cook, Kane
TOTAL:	24.5				

Proposed 2015-2019 Road Program Executive Summary

Option 1A – Maintain Current Pavement Condition (2015-2019)

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2015	\$1,064,000	\$1,064,000
	<i>Meadow Hill Road</i>	<i>(Fox Hunt to Spring Creek Road)</i>
	<i>Haegers Bend Road</i>	<i>(County Line Road to Chapel Road)</i>
	<i>Plum Tree Road</i>	<i>(Ridge Road to Village Limits)</i>
	<i>Cuba Road</i>	<i>(Buckley Road to Merri Oaks Road)</i>
	<i>Cuba Road Bridge (Local Share)</i>	
2016	\$1,123,000	\$1,179,000
	<i>Woodcreek Road</i>	<i>(Dundee Road to End)</i>
	<i>Rebecca Drive</i>	<i>(Old Sutton to End)</i>
	<i>Helm Road</i>	<i>(Algonquin Road to Village Limits)</i>
	<i>Healy Road</i>	<i>(Dundee Road to Penny Road)</i>
	<i>Tamarack Lane</i>	<i>(Old Sutton Road to End)</i>
	<i>Lakeview Lane</i>	<i>(Dundee Road to End)</i>
	<i>Hawthorne Lane</i>	<i>(Route 59 to Otis Road)</i>
	<i>Tricia Lane</i>	<i>(Old Sutton Road to End)</i>
	<i>VH Asphalt Parking Lots</i>	
	<i>Spring Creek Road (Local Share)</i>	<i>W. of Haegers Bend</i>
2017	\$1,162,000	\$1,281,000
	<i>Chapel Road</i>	<i>(Church Road to Haegers Bend)</i>
	<i>Church Road</i>	<i>(Chapel Road to Algonquin Road)</i>
	<i>Haegers Bend Road</i>	<i>(Chapel Road to Algonquin Road)</i>
	<i>River Road</i>	<i>(W. of Haegers Bend to Village Limits)</i>
	<i>River/Algonquin Road</i>	<i>(E. of Braeburn to W. of Church)</i>
2018	\$956,000	\$1,106,000
	<i>Hickory Lane</i>	<i>(Merri Oaks Road to End)</i>
	<i>Ridge Road</i>	<i>(County Line Road to Merri Oaks Road)</i>
	<i>Merri Oaks Road</i>	<i>(Ridge Road to Cuba Road)</i>
	<i>Old Hart Road</i>	<i>(County Line Road to End)</i>
2019	\$998,000	\$1,213,000
	<i>Spring Lane</i>	<i>(Spring Creek Road to End)</i>
	<i>Little Bend Road</i>	<i>(Spring Lane to End)</i>
	<i>Creekside Lane</i>	<i>(Old Sutton Road to End)</i>
	<i>Springwood Lane</i>	<i>(Algonquin Road to End)</i>
Total Expenditure (2015 dollars)- \$5.3 million		
2015-2019 Mileage Resurfaced- 16.99 Miles (3.40 Miles/Year)		
Backlog Reduction- 0.99 Miles		

**Option 2A – Maintain Current Pavement Condition &
Eliminate Backlog in 5 Years (2015-2019)**

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2015	\$1,064,000	\$1,064,000
	<i>Meadow Hill Road</i>	<i>(Fox Hunt to Spring Creek Road)</i>
	<i>Haegers Bend Road</i>	<i>(County Line Road to Chapel Road)</i>
	<i>Plum Tree Road</i>	<i>(Ridge Road to Village Limits)</i>
	<i>Cuba Road</i>	<i>(Buckley Road to Merri Oaks Road)</i>
	<i>Cuba Road Bridge (Local Share)</i>	
2016	\$1,325,000	\$1,392,000
	<i>Woodcreek Road</i>	<i>(Dundee Road to End)</i>
	<i>Rebecca Drive</i>	<i>(Old Sutton to End)</i>
	<i>Helm Road</i>	<i>(Algonquin Road to Village Limits)</i>
	<i>Healy Road</i>	<i>(Dundee Road to Penny Road)</i>
	<i>Tamarack Lane</i>	<i>(Old Sutton Road to End)</i>
	<i>Church Road</i>	<i>(Chapel Road to Algonquin Road)</i>
	<i>Lakeview Lane</i>	<i>(Dundee Road to End)</i>
	<i>Hawthorne Lane</i>	<i>(Route 59 to Otis Road)</i>
	<i>Tricia Lane</i>	<i>(Old Sutton Road to End)</i>
	<i>VH Asphalt Parking Lots</i>	
	<i>Spring Creek Road (Local Share)</i>	<i>W. of Haegers Bend</i>
2017	\$1,525,000	\$1,681,000
	<i>Chapel Road</i>	<i>(Church Road to Haegers Bend)</i>
	<i>Hickory Lane</i>	<i>(Merri Oaks Road to End)</i>
	<i>Haegers Bend Road</i>	<i>(Chapel Road to Village Limits)</i>
	<i>Ridge Road</i>	<i>(County Line Road to Merri Oaks Road)</i>
	<i>River/Algonquin Road</i>	<i>(E. of Braeburn to W. of Church)</i>
	<i>River Road</i>	<i>(W. of Haegers Bend to Village Limits)</i>
2018	\$1,126,000	\$1,303,000
	<i>Spring Lane</i>	<i>(Spring Creek Road to End)</i>
	<i>Little Bend Road</i>	<i>(Spring Lane to End)</i>
	<i>Merri Oaks Road</i>	<i>(Cuba Road to Ridge Road)</i>
	<i>Old Hart Road</i>	<i>(County Line Road to End)</i>
	<i>Springwood Lane</i>	<i>(Algonquin Road to End)</i>
	<i>Healy Road</i>	<i>(Penny Road to End)</i>
2019	\$1,163,000	\$1,413,000
	<i>Creekside Lane</i>	<i>(Algonquin Road to End)</i>
	<i>Butternut Road</i>	<i>(Donlea Road to End)</i>
	<i>Hills & Dales Road</i>	<i>(Brinker Road to Otis Road)</i>
	<i>Cuba Road</i>	<i>(Merri Oaks Road to Plum Tree Road)</i>
	<i>Honeycutt Road</i>	<i>(Hills & Dales Road to End)</i>
	<i>Country Oaks Lane</i>	<i>(Country Oaks Drive to End)</i>
	<i>Country Oaks Drive</i>	<i>(County Line Road to End)</i>
Total Expenditure (2015 dollars)- \$6.2 million		
2015-2019 Mileage Resurfaced- 20.36 Miles (4.07 Miles/Year)		
Backlog Reduction- 4.36 Miles		

Proposed 2015-2021 Road Program Executive Summary

Option 1B – Maintain Current Pavement Condition (2015-2021)

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2015	\$1,072,000	\$1,072,000
	<i>Meadow Hill Road Haegers Bend Road Plum Tree Road</i>	<i>Cuba Road Cuba Road Bridge (Local Share)</i>
2016	\$1,123,000	\$1,179,000
	<i>Woodcreek Road Rebecca Drive Helm Road Healy Road Tamarack Lane</i>	<i>Lakeview Lane Hawthorne Lane Tricia Lane VH Asphalt Parking Lots Spring Creek Road (Local Share)</i>
2017	\$1,162,000	\$1,281,000
	<i>Chapel Road Church Road Haegers Bend Road</i>	<i>River Road River/Algonquin Road</i>
2018	\$956,000	\$1,106,000
	<i>Hickory Lane Ridge Road</i>	<i>Merri Oaks Road Old Hart Road</i>
2019	\$998,000	\$1,213,000
	<i>Spring Lane Little Bend Road</i>	<i>Creekside Lane Springwood Lane</i>
2020	\$975,000	\$1,244,000
	<i>Butternut Road Hills & Dales Road Cuba Road Healy Road</i>	<i>Honeycutt Road Rolling Hills Drive Longmeadow Drive Longmeadow Court</i>
2021	\$962,000	\$1,290,000
	<i>Oak Knoll Road Country Oaks Lane Country Oaks Drive</i>	
Total Expenditure (2015 dollars)- \$7.2 million 2015-2021 Mileage Resurfaced- 23.72 Miles (3.39 Miles/Year) Backlog Reduction- 1.32 Miles		

**Option 2B – Maintain Current Pavement Condition &
Eliminate Backlog in 5 Years (2015-2021)**

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2015	\$1,064,000	\$1,064,000
	<i>Meadow Hill Road Haegers Bend Road Plum Tree Road</i>	<i>Cuba Road Cuba Road Bridge (Local Share)</i>
2016	\$1,325,000	\$1,392,000
	<i>Woodcreek Road Rebecca Drive Helm Road Healy Road Tamarack Lane Church Road Lakeview Lane</i>	<i>Hawthorne Lane Tricia Lane Church Road VH Asphalt Parking Lots Spring Creek Road (Local Share)- West of Haegers Bend</i>
2017	\$1,525,000	\$1,681,000
	<i>Chapel Road Hickory Lane Haegers Bend Road</i>	<i>Ridge Road River/Algonquin Road River Road</i>
2018	\$1,126,000	\$1,303,000
	<i>Spring Lane Little Bend Road Merri Oaks Road</i>	<i>Old Hart Road Springwood Lane Healy Road</i>
2019	\$1,163,000	\$1,413,000
	<i>Creekside Lane Butternut Road Hills & Dales Road Cuba Road</i>	<i>Honeycutt Road Country Oaks Lane Country Oaks Drive</i>
2020	\$1,037,000	\$1,324,000
	<i>Rolling Hills Drive Longmeadow Court</i>	<i>Longmeadow Drive Oak Knoll Drive</i>
2021	\$951,000	\$1,275,000
	<i>Surrey Lane East Surrey Lane West Surrey Court Wagon Wheel Lane</i>	<i>Rock Ridge Road Hawley Woods Road Round Barn Road Old Bartlett Road</i>
Total Expenditure (2015 dollars)- \$8.19 Million 2015-2021 Mileage Resurfaced- 27.05 Miles (3.86 Miles/Year) Backlog Reduction- 4.65 Miles		

Proposed 2015-2024 Road Program Executive Summary

Option 1C – Maintain Current Pavement Condition (2015-2024)

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2015	\$1,072,000	\$1,072,000
	<i>Meadow Hill Road Haegers Bend Road Plum Tree Road</i>	<i>Cuba Road Cuba Road Bridge (Local Share)</i>
2016	\$1,123,000	\$1,179,000
	<i>Woodcreek Road Rebecca Drive Helm Road Healy Road Tamarack Lane</i>	<i>Lakeview Lane Hawthorne Lane Tricia Lane VH Asphalt Parking Lots Spring Creek Road (Local Share)</i>
2017	\$1,162,000	\$1,281,000
	<i>Chapel Road Church Road Haegers Bend Road</i>	<i>River Road River/Algonquin Road</i>
2018	\$956,000	\$1,106,000
	<i>Hickory Lane Ridge Road</i>	<i>Merri Oaks Road Old Hart Road</i>
2019	\$998,000	\$1,213,000
	<i>Spring Lane Little Bend Road</i>	<i>Creekside Lane Springwood Lane</i>
2020	\$975,000	\$1,244,000
	<i>Butternut Road Hills & Dales Road Cuba Road Healy Road</i>	<i>Honeycutt Road Rolling Hills Drive Longmeadow Drive Longmeadow Court</i>
2021	\$962,000	\$1,290,000
	<i>Oak Knoll Road Country Oaks Lane</i>	<i>Country Oaks Drive</i>
2022	\$851,000	\$1,197,000
	<i>Surrey Lane East Surrey Lane West Surrey Court Wagon Wheel Lane</i>	<i>Rock Ridge Road Hawley Woods Road Round Barn Road</i>
2023	\$1,021,000	\$1,508,000
	<i>Old Dundee Road Village Hall Concrete Parking Lot</i>	<i>Braeburn Road Cross Timbers Road</i>
2024	\$962,000	\$1,493,000
	<i>Buckley Road Leeds Drive Crabapple Road Aberdeen Drive</i>	<i>Dundee Lane Caesar Drive Dana Lane</i>
Total Expenditure (2015 dollars)- \$10.1 million		
2015-2024 Mileage Resurfaced- 32.99 Miles (3.29 Miles/Year)		
Backlog Reduction- 0.99 Miles		

**Option 2C – Maintain Current Pavement Condition &
Eliminate Backlog in 5 Years (2015-2024)**

Year	Preliminary Budget (2015 dollars)*	Adjusted Preliminary Budget
2015	\$1,064,000	\$1,064,000
	<i>Meadow Hill Road Haegers Bend Road Plum Tree Road</i>	<i>Cuba Road Cuba Road Bridge (Local Share)</i>
2016	\$1,325,000	\$1,392,000
	<i>Woodcreek Road Rebecca Drive Helm Road Healy Road Tamarack Lane Church Road Lakeview Lane</i>	<i>Hawthorne Lane Tricia Lane Church Road VH Asphalt Parking Lots Spring Creek Road (Local Share)- West of Haegers Bend</i>
2017	\$1,525,000	\$1,681,000
	<i>Chapel Road Hickory Lane Haegers Bend Road</i>	<i>Ridge Road River/Algonquin Road River Road</i>
2018	\$1,126,000	\$1,303,000
	<i>Spring Lane Little Bend Road Merri Oaks Road</i>	<i>Old Hart Road Springwood Lane Healy Road</i>
2019	\$1,163,000	\$1,413,000
	<i>Creekside Lane Butternut Road Hills & Dales Road Cuba Road</i>	<i>Honeycutt Road Country Oaks Lane Country Oaks Drive</i>
2020	\$1,037,000	\$1,324,000
	<i>Rolling Hills Drive Longmeadow Court</i>	<i>Longmeadow Drive Oak Knoll Drive</i>
2021	\$951,000	\$1,275,000
	<i>Surrey Lane East Surrey Lane West Surrey Court Wagon Wheel Lane</i>	<i>Rock Ridge Road Hawley Woods Road Round Barn Road Old Bartlett Road</i>
2022	\$927,000	\$1,304,000
	<i>Old Dundee Road Village Hall Concrete Parking Lot Buckley Road</i>	<i>Leeds Drive Crabapple Road Aberdeen Road</i>
2023	\$1,056,000	\$1,560,000
	<i>Braeburn Road Cross Timbers Road Dundee Lane</i>	<i>Caesar Drive Dana Lane</i>
2024	\$1,035,000	\$1,606,000
	<i>Barrington Hills Road Spring Creek Road</i>	
Total Expenditure (2015 dollars)- \$11.2 Million		
2015-2024 Mileage Resurfaced- 36.55 Miles (3.66 Miles/Year)		
Backlog Reduction- 4.55 Miles		

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 14, 2015

Re: Otis Road Preconstruction meeting

On Wednesday, August 5, 2015 I attended a preconstruction meeting at the offices of the Cook County Highway Department for the County's 2015 Pavement Preservation (North) Project. The project includes several roads at various locations throughout the county, including the resurfacing of Otis Road between Dundee Avenue and Brinker Road. The following are a summary of relevant issues discussed at the meeting:

- Otis Road is one of two roads that is required to be completed by November 25th. The other roads in the project have a July 2016 completion date.
- The anticipated start date for the project is Monday, August 31st, pending issuance of a notice to proceed and a completion of the contract paperwork.
- Based on the condition of Otis Road west of Brinker I requested that the County consider patching beyond the western project limits if fund allow. The County did not commit but indicated they would consider this request.

After the meeting I discussed with County design staff the concerns of the Village relative to Brinker Road and the remainder of Otis Road and asked if consideration had been given to including these roads in the 2016 program. County staff indicated that the design of the 2016 program was underway and currently it does not include either of these roadways. We have requested a meeting with the Superintendent of Highways to review the County's resurfacing schedule.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 14, 2015

Re: Functional Classification System Request
Ridge Road & Plum Tree Road

Based on previous direction from the Roads & Bridges Committee our office submitted a request in May to the McHenry County Council of Mayors (MCCOM) to revise the functional classification of Ridge Road and Plum Tree east of Ridge Road from "Minor Arterial" to Major Collector". A response was received from MCCOM that requested, among other items, that a letter of support be provided by the Village of Lake Barrington for the Plum Tree Road reclassification as the municipal boundaries of Lake Barrington abut Plum Tree Road.

We provided a draft letter of support for the consideration of the Village of Lake Barrington. Upon review of the information, we received a response from the Village Administrator stating that the Village of Lake Barrington does not support the proposed reclassification, citing current traffic volumes as well as testimony provided by Barrington Hills residents at recent public meetings noting significant congestion at the intersection and the functionality of Plum Tree Road as an arterial route between several communities in the Barrington area.

As discussed previously, reducing the classification of these roads to Major Collector would not change their eligibility status for federal funding or have any other direct impacts to the Village of Barrington Hills. If the Village intends to pursue these requests further some additional coordination with the Village of Lake Barrington appears to be necessary.

MEMORANDUM

To: Robert Kosin, VBH Director of Administration
Brian Cecola, VBH Chairman Roads & Bridges

From: Dan Strahan, P.E., CFM
Gewalt Hamilton Associates (GHA)

Date: August 14, 2015

Re: Lakeview Lane Drainage

On Friday, August 14, 2015 Village staff met with Mr. John Keady to discuss the status of potential drainage improvements along Lakeview Lane. A conceptual storm sewer plan had been prepared and is attached to this memo for reference. Mr. Kosin proposed to Mr. Keady that the portion of storm sewer between Lakeview Lane and Heather Lake be privately funded and constructed with an easement to allow for future access by the Village. The Village would then install the remaining storm sewer within the Lakeview Lane right-of-way and make appropriate ditch grading improvements to connect to the storm sewer along Mr. Keady's lot line.

Mr. Keady indicated he would discuss with his neighbors the possibility of privately funding the portion of the storm sewer on his property. He requested that the Village review any potential interim drainage improvements (clearing ditchlines, etc.) to minimize the amount of ponding in the ditches.

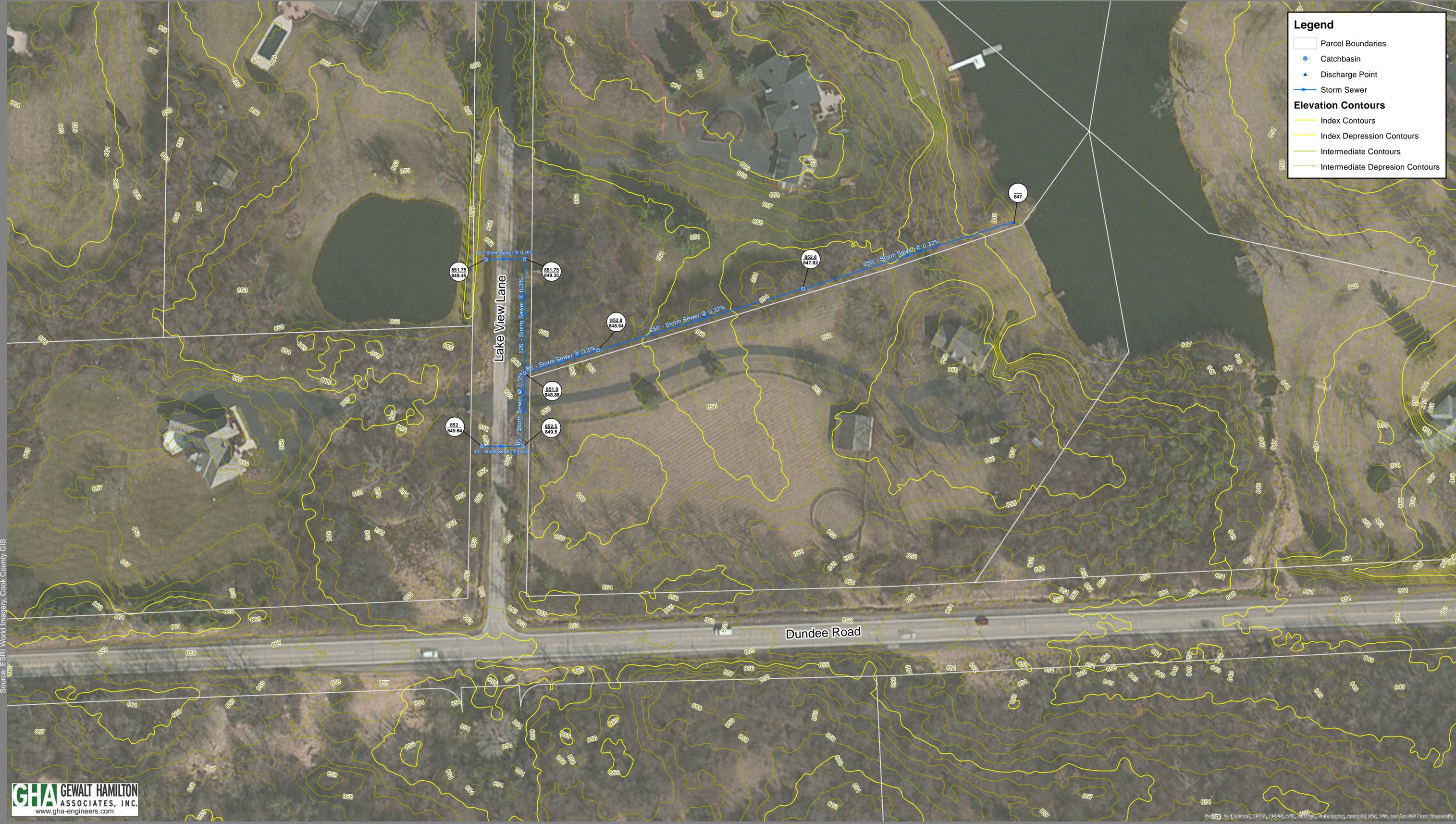
Pending upcoming budget discussions, it is anticipated that Lakeview Lane may be included in the scope of the 2016 Road Program which would be the ideal time to complete the public portion of the storm sewer improvements noted above. In addition, the Village will review the potential to complete ditch grading and clearing operations this fall to help reduce to the extent possible the duration of ponding within the roadside ditches during and after future storm events.

Legend

- Parcel Boundaries
- Catchbasin
- Discharge Point
- Storm Sewer

Elevation Contours

- Index Contours
- Index Depression Contours
- Intermediate Contours
- Intermediate Depression Contours



Source: ESRI World Imagery, Cook County GIS

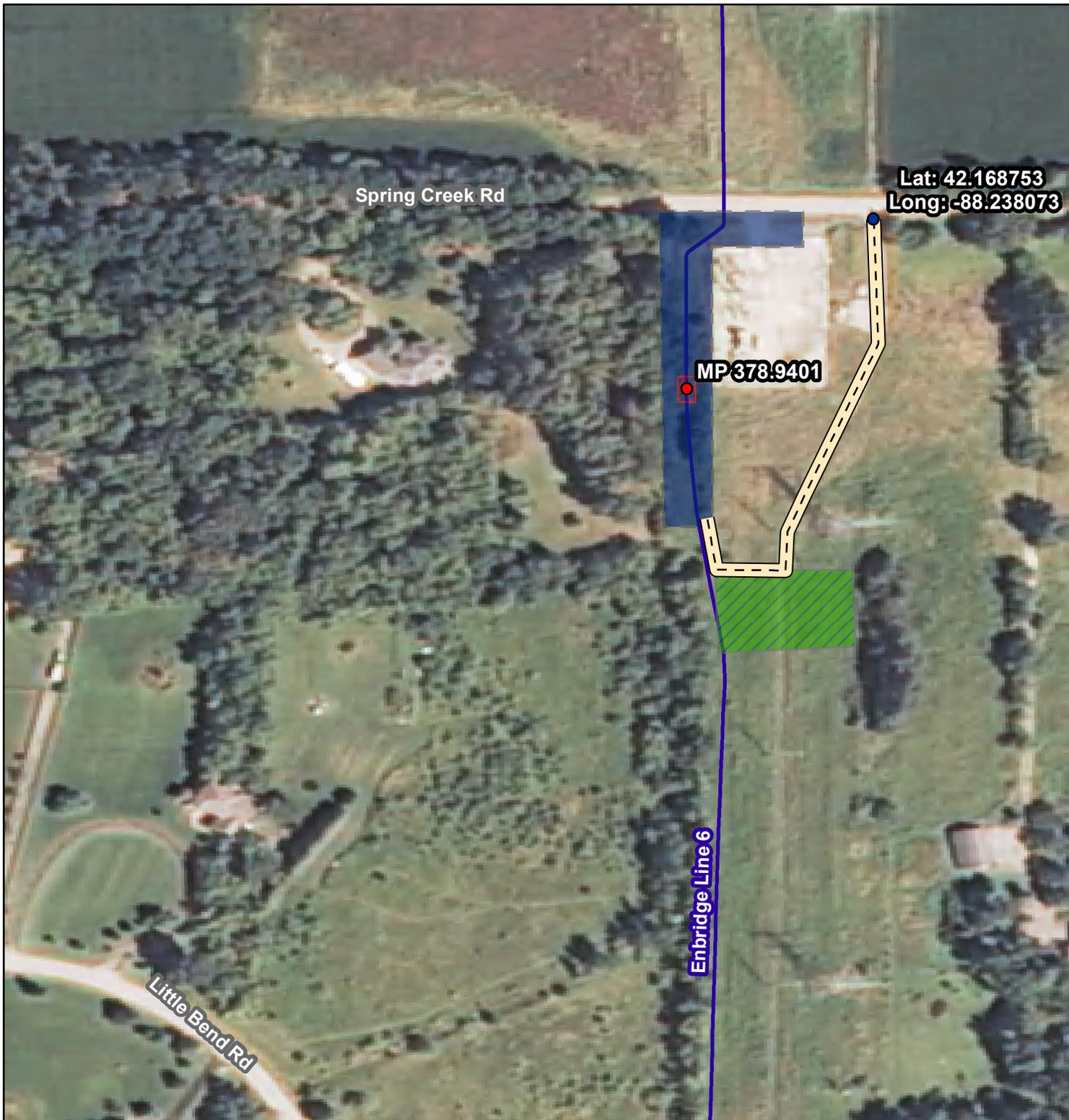
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Drawn By: gnewton



1 inch = 50 Feet

Storm Sewer Map
Barrington Hills, IL



- Access Location
- Target Girthweld
- Line 6
- Access Route
- Excavation Limits
- Temporary Work Space
- Staging and Spoil Area



INTEGRITY DIG ACCESS PLAN

Line 6A
 Mile Post 378.9401
 McHenry County, IL

